



MEMORANDUM

TO: Milton Dohoney, Jr., City Administrator
FROM: Nicholas Hutchinson, City Engineer
Brian Steglitz, Interim Public Services Area Administrator
DATE: December 15, 2022
SUBJECT: E. Medical Center Drive Bridge Pathway

This memorandum is in response to Resolution [R-21-395](#), which included the following directive:

“RESOLVED, That Council directs the City Administrator to negotiate an amendment to the contract with DLZ to design the east-west pathway connection under the East Medical Center Drive Bridge and develop a cost estimate for construction, and that the City Administrator be authorized to approve this amendment to the contract;”

The design work for this pathway has now been completed. Attached is a memo from the City’s design consultant, DLZ, which includes the project plans and an estimate of construction costs. Based on their estimate, the total cost of the pathway project is anticipated to be approximately \$340,000 in 2023 dollars.

When originally contemplated, there was a possibility that this pathway project could be included as part of the construction of the East Medical Center Drive Bridge Rehabilitation and Widening Project. However, during the design it was discovered that a portion of the platform beneath the existing bridge is within the railroad right-of-way.

The Michigan Department of Transportation (MDOT), who owns the railroad, was contacted, but they were not willing to grant the necessary easement to construct the pathway at this time. Thus, the construction of the pathway has not been included as part of the bridge project. However, MDOT was willing to allow the widening of the platform as part of the current bridge project. This work will be included and will prepare the space beneath the bridge for the future pathway project. More details can be found in the attached DLZ memo.

The pathway project is included in the City’s Capital Improvements Plan (TR-AT-16-04) and will be reevaluated for construction in a future year after MDOT has conducted their trespass prevention study. It should also be noted that a funding source for the construction of the pathway has yet to be identified.

The construction contract for the East Medical Center Drive Bridge Rehabilitation and Widening Project, as well as a contract for construction management services for the project, are scheduled to come before Council on January 9, 2023. More information on that project will be available in those documents, as well as Resolution R-21-395.

cc: Francisca Chan, Engineering Project Manager
Raymond Hess, Transportation Manager



INNOVATIVE IDEAS
EXCEPTIONAL DESIGN
UNMATCHED CLIENT SERVICE

PATHWAY MEMORANDUM

DATE: May 12, 2022
TO: City of Ann Arbor
FROM: DLZ Michigan, Inc.
SUBJECT: Ann Arbor – Pathway Under E. Medical Center Dr. Bridge

As part of DLZ’s design engineering work, we reached out to the Michigan Department of Transportation (MDOT) to determine the need for an easement or licensing agreement for the proposed pathway under the E. Medical Center Dr. bridge, south of Fuller Rd. Due to the proximity of the MDOT right-of-way (ROW) with the north bridge abutment, any pathway constructed below the bridge will require MDOT’s approval prior to construction. Please see attached cross-section exhibit to see that the pathway would need to be at least partially within MDOT’s ROW. After some discussion and several emails back and forth, it was decided by MDOT that the City of Ann Arbor cannot construct the pathway within their ROW at this time.

MDOT expressed concerns regarding the number of trespassing issues they have experienced in the Ann Arbor area, including trespassing issues with the existing Gallup Park trail adjacent to the railroad and its associated fencing.

Additionally, MDOT made us aware of an upcoming corridor wide trespass prevention grant they will be receiving. As such, they are planning to take a more comprehensive look at the entire Ann Arbor corridor to ensure consistency, connectivity, and ultimately promote safety within the railroad ROW.

MDOT informed us the proposed pathway will be included in a larger discussion regarding pedestrian access to MDOT Rail ROW in Ann Arbor. They will be reaching out to the appropriate stakeholders in the near future to start the discussion and plan to establish a kickoff meeting at that time. The design work for the pathway is being completed as part of the E. Medical Center Dr. Bridge Widening project.

MDOT is allowing the City to move forward with construction on the structure, including widening the existing 10 ft flat concrete pad under the bridge, behind the north bridge pier crashwall which is within the MDOT ROW. DLZ is currently designing the improvements to widen the existing horizontal section from 10 feet wide to 14 feet wide in anticipation of connecting the proposed pathway to this area in the future.

Unfortunately, with the additional coordination required with MDOT prior to construction and the current schedule for the project, DLZ will not be able to include the pathway in the bridge project and keep the project on schedule for construction in 2023. MDOT not providing an easement or agreeing to a licensing agreement at this time is the primary reason we cannot include it with the E. Medical Center Dr. Bridge Widening project.

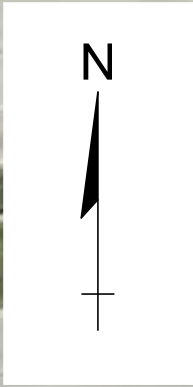


INNOVATIVE IDEAS
EXCEPTIONAL DESIGN
UNMATCHED CLIENT SERVICE

Title
Subtitle
Page 2 of 2

DLZ will complete the design and cost estimate of the pathway but present it as a separate project that can be constructed in the future once MDOT has provided their approval and agreed to provide an easement or licensing agreement.





FULLER RD

FULLER RD

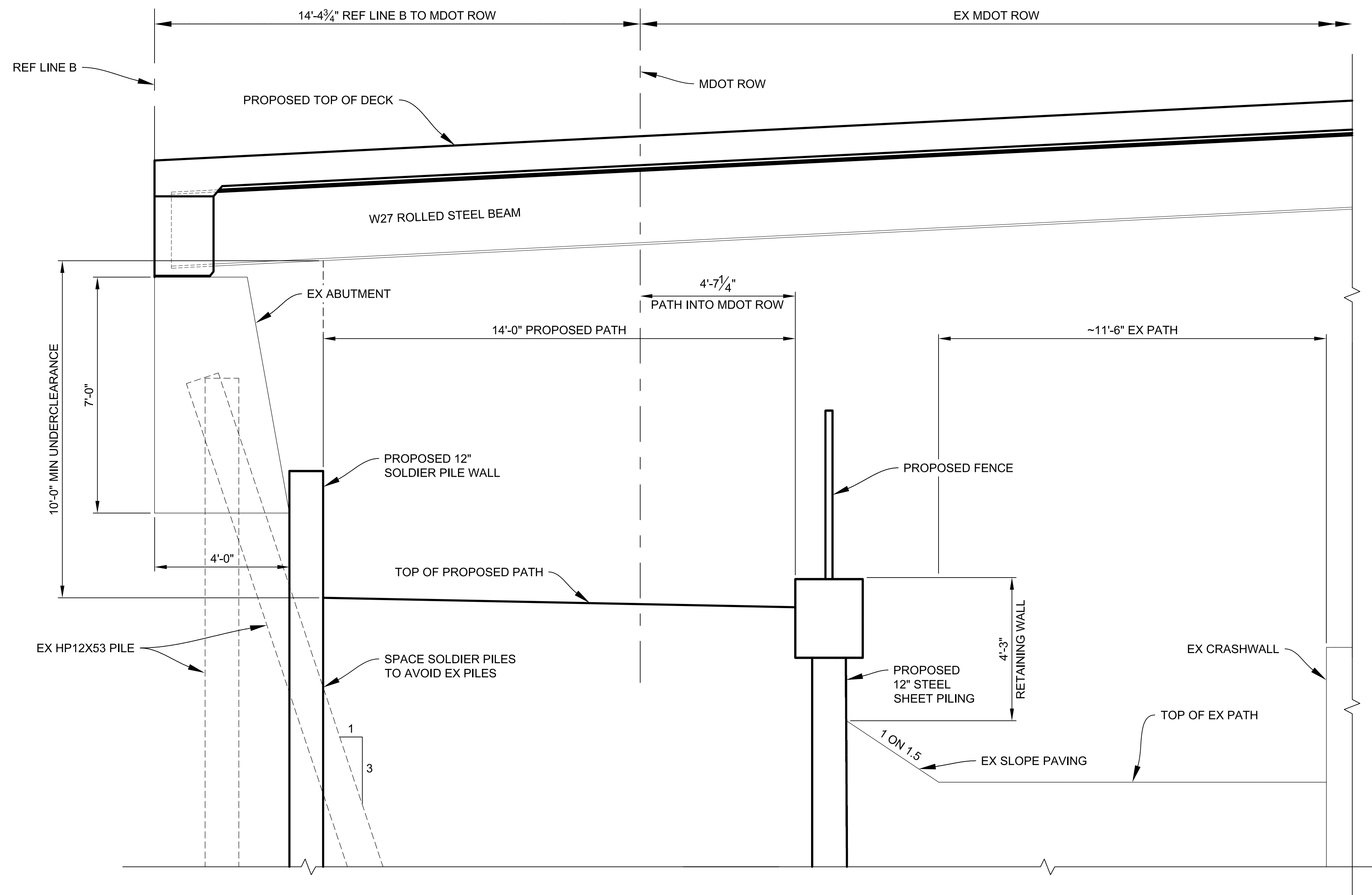
PROPOSED MULTI-USE
PATH UNDER BRIDGE

EAST MEDICAL
CENTER DR

MULTI-USE PATH
EXHIBIT
12/8/21

0 40
HORZ. (FT)





PATH SECTION

NOTES:

THERE IS A VERY HIGH LIKELIHOOD THAT THIS WORK WOULD REQUIRE A COMPLETE ABUTMENT REPLACEMENT, GOING FROM THE SHORT CURTAINWALL ABUTMENT TO A TALL CANTILEVERED ABUTMENT ON PILES. THE EXISTING BATTERED PILES RELY ON THE SLOPE ABOVE THEM TO PROVIDE CAPACITY. EVEN IF THERE IS NO DIRECT CONFLICT WITH THE PILES THEMSELVES, THE COVER WOULD BE REDUCED WHICH IN TURN WOULD REDUCE PILE CAPACITY. ADDITIONAL MEASURES WOULD NEED TO BE TAKEN TO RESTORE THAT CAPACITY. THERE IS A POSSIBILITY THAT MICROPILES COULD BE DRILLED THROUGH THE ABUTMENT, BUT MICROPILES ARE A SLENDER ELEMENT AND WOULD LIKELY NOT ADD ENOUGH BENDING RESISTANCE TO BE COMPARABLE TO THE EXISTING BATTERED PILES NEEDED FOR THIS RIGID FOUNDATION. THE ABUTMENT WOULD PROBABLY NEED TO BE RECONSTRUCTED.



PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

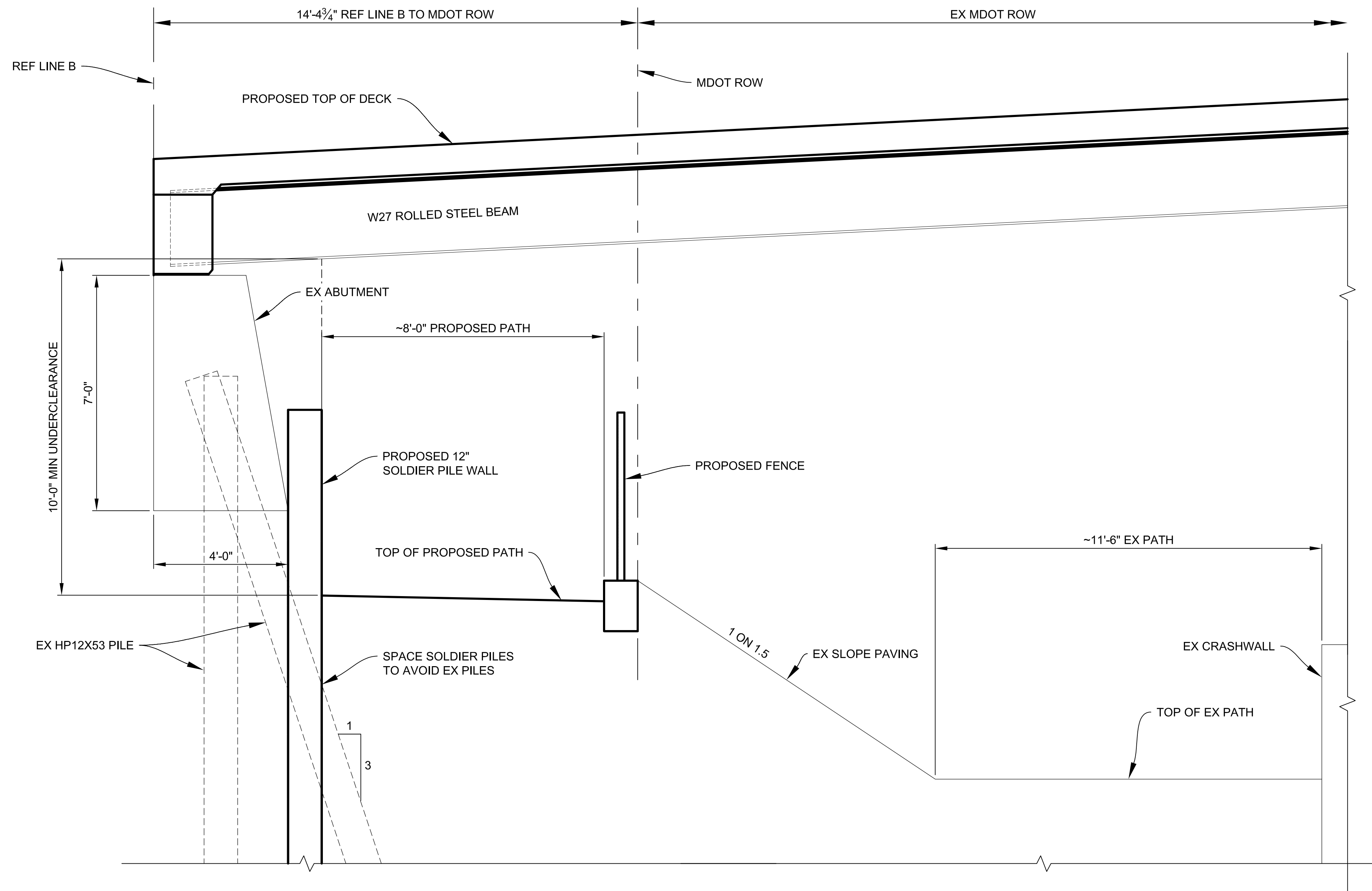
EAST MEDICAL CENTER DRIVE

PATH TYPICAL SECTION

SCALE	INCH
HOR. 1/2"=1'-0"	0 2
DRAWING NO.	XXX XXX
SHEET NO.	___ OF ___

REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY

X:\Projects\2021\2141\736300_AA - EMCD Bridge\01_Constr\Docs\CAD\Exhibits\EMCD_Path_Section



PATH SECTION

NOTES:

THERE IS A VERY HIGH LIKELIHOOD THAT THIS WORK WOULD REQUIRE A COMPLETE ABUTMENT REPLACEMENT, GOING FROM THE SHORT CURTAINWALL ABUTMENT TO A TALL CANTILEVERED ABUTMENT ON PILES. THE EXISTING BATTERED PILES RELY ON THE SLOPE ABOVE THEM TO PROVIDE CAPACITY. EVEN IF THERE IS NO DIRECT CONFLICT WITH THE PILES THEMSELVES, THE COVER WOULD BE REDUCED WHICH IN TURN WOULD REDUCE PILE CAPACITY. ADDITIONAL MEASURES WOULD NEED TO BE TAKEN TO RESTORE THAT CAPACITY. THERE IS A POSSIBILITY THAT MICROPILES COULD BE DRILLED THROUGH THE ABUTMENT, BUT MICROPILES ARE A SLENDER ELEMENT AND WOULD LIKELY NOT ADD ENOUGH BENDING RESISTANCE TO BE COMPARABLE TO THE EXISTING BATTERED PILES NEEDED FOR THIS RIGID FOUNDATION. THE ABUTMENT WOULD PROBABLY NEED TO BE RECONSTRUCTED.



PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

EAST MEDICAL CENTER DRIVE

PATH TYPICAL SECTION

SCALE	INCH
HOR. 1/2"=1'-0"	0 2
DRAWING NO.	XXX XXX
SHEET NO.	___ OF ___

REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY

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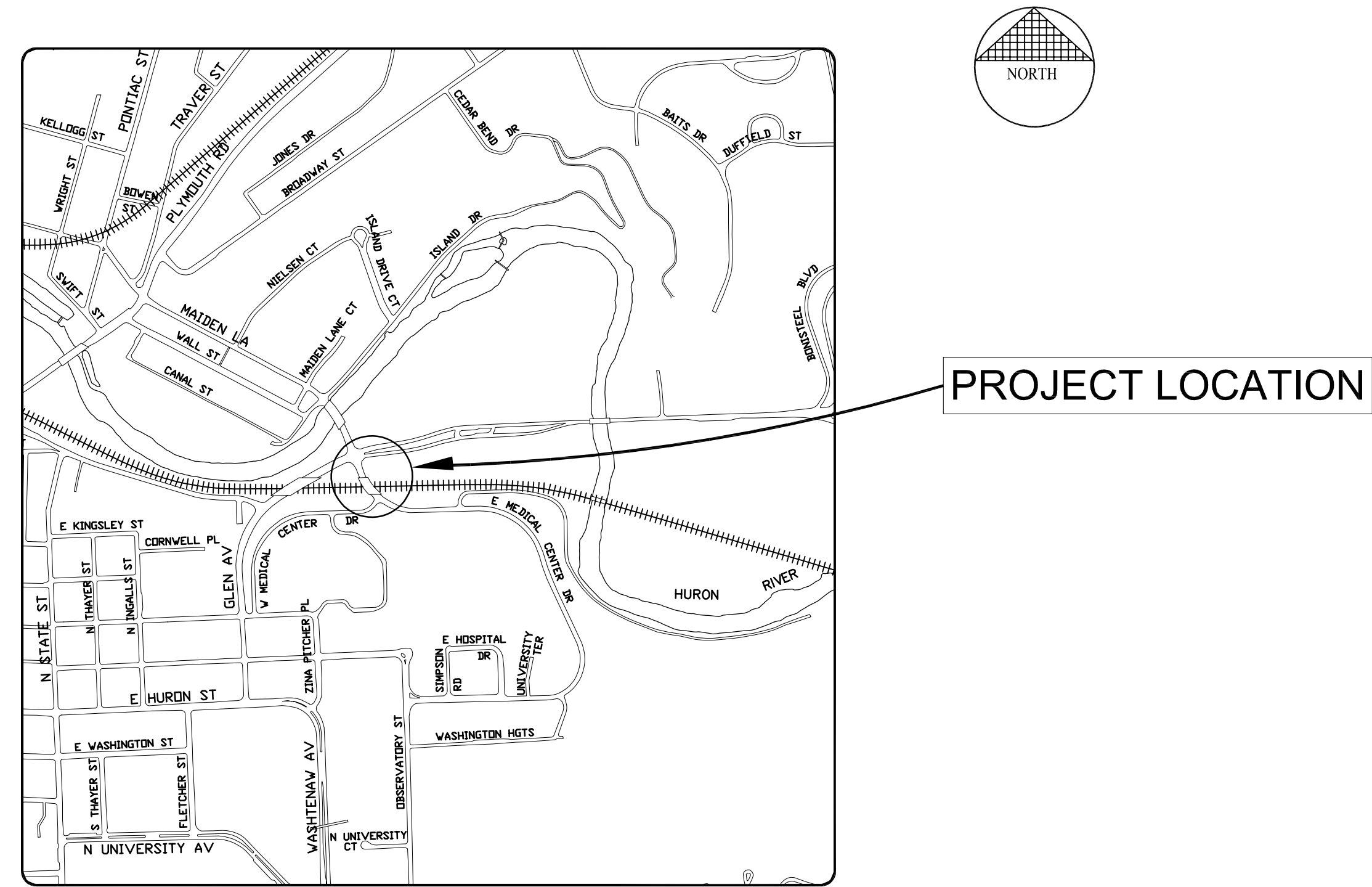
NOTES:

EXCEPT WHERE OTHERWISE INDICATED ON THESE PLANS, OR IN THE CONTRACT DOCUMENTS AND DETAILED SPECIFICATIONS CONTAINED HEREIN, ALL MATERIALS AND WORKMANSHIP SHALL BE ACCORDING TO THE LATEST REVISIONS OF THE CITY OF ANN ARBOR PUBLIC SERVICE DEPARTMENT STANDARD SPECIFICATIONS AND THE MICHIGAN DEPARTMENT OF TRANSPORTATION (MDOT) STANDARD SPECIFICATIONS FOR CONSTRUCTION 2020 EDITION AS APPLICABLE.

CITY OF ANN ARBOR ENGINEERING

E. MEDICAL CENTER DRIVE PATHWAY

PERMITS REQUIRED TO BE OBTAINED BY THE CONTRACTOR PRIOR TO THE BEGINNING OF CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR THESE PERMIT FEES.	
PERMIT	ISSUING AUTHORITY
GRADING/ SOIL EROSION & SEDIMENTATION CONTROL PERMIT	CITY OF ANN ARBOR PLANNING AND DEVELOPMENT SERVICES UNIT
AMTRAK ENTRY PERMIT	AMTRAK



CITY FILE NO. XXXX-XXX

CONTRACT FOR: PATHWAY INSTALLATION UNDER THE E. MEDICAL CENTER DRIVE BRIDGE.

PREPARED UNDER THE SUPERVISION OF	SEAL
LEIGH MERRILL P.E., MICHIGAN LICENSE NO. _____	DATE _____
DRAWING NO. C1	SHEET 1 OF 10

3 WORKING DAYS | BEFORE YOU DIG CALL MISS DIG 800-482-7171 (TOLL FREE)

FOR PROTECTION OF UNDERGROUND UTILITIES AND IN CONFORMANCE WITH PUBLIC ACT 53, THE CONTRACTOR SHALL DIAL 1-800-482-7171 A MINIMUM OF THREE FULL WORKING DAYS, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO BEGINNING EACH EXCAVATION IN AREAS WHERE PUBLIC UTILITIES HAVE NOT BEEN PREVIOUSLY LOCATED. MEMBERS WILL THUS BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE "MISS DIG" ALERT SYSTEM.

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© E. MEDICAL CENTER DRIVE PATHWAY

NOTES:

1. THE UNDERGROUND LOCATIONS AND DEPTH SHOWN FOR ALL PUBLIC AND PRIVATE UTILITIES ARE APPROXIMATE. THE CITY OF ANN ARBOR AND DLZ ASSUME NO RESPONSIBILITY FOR THEIR ACCURATE REPRESENTATION IN THIS DRAWING. THE CONTRACTOR SHALL CONTACT MISS DIG TO LOCATE ALL MARKED AND UNMARKED UTILITIES PRIOR TO STARTING WORK AND SHALL CONDUCT HIS OPERATIONS IN SUCH A MANNER AS TO ENSURE THAT THOSE UTILITIES NOT REQUIRING RELOCATION WILL NOT BE DISTURBED.
2. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ANY DELAYS, DAMAGES, COSTS AND/OR CHARGES INCURRED DUE TO AND/OR BY REASON OF ANY UTILITY, STRUCTURE, FEATURES AND/OR SITE CONDITION, WHETHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL REPAIR AND/OR REPLACE, AT ITS SOLE EXPENSE, TO AS GOOD OR BETTER CONDITION, ANY AND ALL UTILITIES, STRUCTURES, FEATURES AND/OR SITE CONDITIONS WHICH ARE IMPACTED BY REASON OF THE WORK, OR INJURED BY ITS OPERATIONS, OR DURING THE OPERATIONS OF ITS SUBCONTRACTORS OR SUPPLIERS.
3. DRIVEWAYS AND ENTRANCES TO BUILDINGS, REAL PROPERTY, AND THE LIKE SHALL NOT BE BLOCKED EXCEPT FOR SHORT DURATIONS AND ONLY WHEN APPROVED BY THE ENGINEER, VEHICULAR AND PEDESTRIAN ACCESS SHALL BE MAINTAINED AT ALL TIMES AS SPECIFIED IN MAINTAINING TRAFFIC PLANS AND SPECIFICATIONS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTINUOUS MAINTENANCE OF THE TEMPORARY TRAFFIC CONTROL DEVICES AND SOIL EROSION CONTROL MEASURES WITHIN THE CONSTRUCTION AREA AND APPROACHES TO PROJECT SITE, UNTIL THE FINAL COMPLETION OF THE PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE RELATED ITEMS OF WORK.
5. THE LOCATION OF MATERIAL STOCK PILES AND ON SITE STAGING AREAS ARE TO BE APPROVED BY THE ENGINEER. ANY MATERIALS STOCKPILED OR STORED SHALL NOT CREATE AN EROSION PROBLEM, INTERFERE WITH DRAINAGE, OR CREATE INSUFFICIENT SIGHT DISTANCE.
6. EXISTING STREET NAME, GUIDE, REGULATORY SIGNS, BRIDGE LIGHTING, ETC, WHICH CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED PRIOR TO CONSTRUCTION, STORED IN A MANNER WHICH WILL PREVENT DAMAGE, AND RE-SET IN LOCATIONS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE OTHER BID ITEMS OF WORK.
7. WHERE STREET CURBS ARE UNDERMINED DUE TO CONSTRUCTION ACTIVITIES, THEY SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER.

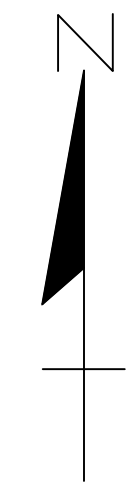
GENERAL:

NOTIFY THE CITY OF ANN ARBOR SOIL EROSION CONTROL OFFICE 48 HOURS PRIOR TO BEGINNING WORK ON THE PROJECT. PHONE: 734-794-6265.

1. THE CONTRACTOR SHALL IMPLEMENT AND MAINTAIN THE SOIL EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER AT ALL TIMES DURING CONSTRUCTION. ANY MODIFICATIONS OR ADDITIONS TO THE SOIL EROSION CONTROL MEASURES DUE TO CONSTRUCTION OR CHANGED CONDITIONS SHALL BE AS DIRECTED AND APPROVED BY THE ENGINEER.
2. ALL SOIL EROSION AND SEDIMENTATION CONTROL WORK SHALL CONFORM TO THE PERMIT REQUIREMENTS OF THE CITY OF ANN ARBOR, CHAPTER 55 ANN ARBOR UNIFIED DEVELOPMENT CODE, CITY OF ANN ARBOR STANDARDS DIVISION VII, THE LAWS OF THE STATE OF MICHIGAN, AND THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
3. DAILY, OR AFTER ANY STORM EVENT, INSPECTIONS OF EROSION CONTROL MEASURES SHALL BE MADE BY THE CONTRACTOR. PERIODIC INSPECTIONS MAY BE MADE BY THE ENGINEER TO DETERMINE THE EFFECTIVENESS OF EROSION AND SEDIMENTATION CONTROL MEASURES. ANY NECESSARY CORRECTIONS SHALL BE MADE WITHOUT DELAY, AND WITHOUT ADDITIONAL COST TO THE CITY OF ANN ARBOR.
4. EROSION AND SEDIMENTATION FROM WORK ON THE SITE SHALL BE CONTAINED ON THE SITE AND NOT BE ALLOWED TO COLLECT ON ANY OFF-SITE AREAS, ROADWAYS OR WATERWAYS.
5. ALL MUD/SOIL TRACKED ONTO ROADWAYS FROM THE SITE DUE TO CONSTRUCTION, SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR. IF SO ORDERED, THE CONTRACTOR SHALL PROVIDE AND OPERATE A VACUUM-TYPE STREET SWEEPER, AT NO ADDITIONAL COST TO THE CITY OF ANN ARBOR.
6. RESTORATION OF ALL DISTURBED AREAS, INCLUDING PLACEMENT OF TOPSOIL, SEED, FERTILIZER AND MULCH AND/OR SOIL SHALL BE PERFORMED WITHIN FIVE (5) DAYS OF THE COMPLETION OF FINAL GRADE.
7. CONSTRUCTION OPERATIONS SHALL BE SCHEDULED AND PERFORMED SO THAT PREVENTATIVE SOIL EROSION CONTROL MEASURES ARE IN PLACE PRIOR TO EXCAVATION IN CRITICAL AREAS AND TEMPORARY STABILIZATION MEASURES ARE IN PLACE IMMEDIATELY FOLLOWING BACKFILLING OPERATIONS.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TEMPORARY SOIL EROSION CONTROL MEASURES AND REMOVAL OF SOME MEASURES UPON AUTHORIZED COMPLETION OF THE PROJECT. FINAL COMPLETION OF PROJECT WILL NOT BE AUTHORIZED UNTIL ALL SITE WORK AND UTILITY CONSTRUCTION IS COMPLETE AND ALL SOILS ARE STABILIZED.

PUBLIC UTILITY CONTACT INFORMATION		
WATER	CITY OF ANN ARBOR FIELD OPERATIONS SERVICE UNIT W.R. WHEELER SERVICE CENTER 4251 STONE SCHOOL ROAD ANN ARBOR, MI 48108	DAN WOODEN (734) 794-6350
SANITARY		TRAVIS CONLEY (734) 794-3304
STORM		KEVIN SCHNEIDER (734) 794-6350
SIGNS SIGNALS STREET LIGHTS		MARC MORENO (734) 794-6350

SHEET INDEX		
SHEET NUMBER	DRAWING NUMBER	SHEET TITLE
1	C1	TITLE SHEET
2	C2	GENERAL NOTES AND QUANTITIES
3	TS1	EXISTING CONDITIONS
4	SB1	SOIL BORINGS
5	R1	PROPOSED SITE PLAN AND PROFILE
6	GD1	DETAIL GRADES
7-9	GD2-GD4	PATHWAY CROSS SECTIONS
10	STD1	STANDARD DETAILS



PRIVATE UTILITY CONTACT INFORMATION		
GAS	DTE ENERGY 3150 E. MICHIGAN AVE YPSILANTE TOWNSHIP, MI 48198	ROBERT CZAPIEWSKI (734) 544-7818
ELECTRIC	DTE ELECTRIC	RAFAEL RAMIREZ-INCIARTE (313)597-4411 rafael.ramirezinciart2@dteenergy.com
CABLE	COMCAST 27800 FRANKLIN ROAD SOUTHFIELD, MI 48034	JEFF DOBIES (734) 359-1669
PHONE	AT&T 550 S. MAPLE ROAD ANN ARBOR, MI 48103	MICHAEL ZAREMA (734) 277-4855
FIBER OPTIC	VERIZON BUSINESS/MCI 3 PARKLANE BOULEVARD DEARBORN, MI 48126	MARLON REDD (313) 588-0849 marlon.redd@verizon.com
FIBER OPTIC	FIBER LINK, INC.	JOEL JARVIS Missdigdsg@fiberinc.com
FIBER OPTIC	WINDSTREAM	JEFF WEBB (734) 790-6556 jeff.webb@windstream.com
FIBER OPTIC	LUMEN CENTURY-LINK 19675 WEST 10 MILE ROAD SOUTHFIELD, MI 48075	DAVE HUCKFELDT (517) 812-2592 dave.huckfeldt@lumen.com
ITS/FIBER OPTIC	UNIVERSITY OF MICHIGAN	KEVIN MCLAUGHLIN (734) 615-5699 mckevin@umich.edu

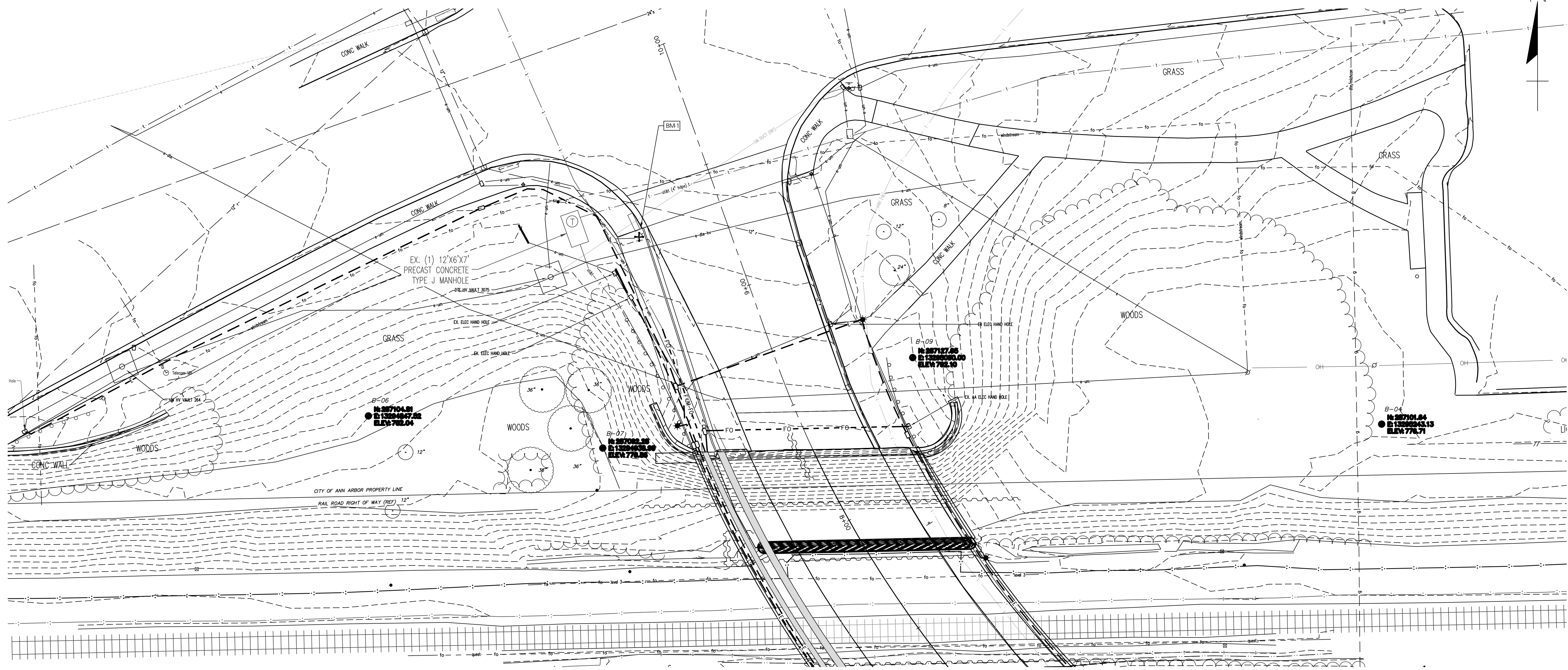
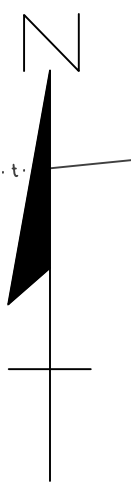
QUANTITY SUMMARY		
DESCRIPTION	UNITS	QUANTITY
Mobilization, Max	LSUM	1
Fence, Rem	Ft	217
Fence, Chain Link, Vinyl Coated	FT	529
Shared Use Path, Aggregate	Ton	153
Shared Use Path, Grading	Ft	590
Shared Use Path, HMA	Ton	153
Excavation, Earth	Cyd	372
Embankment, CIP	Cyd	535
Soil Erosion, Inlet Protection	Ea	1
Soil Erosion, Silt Fence	Ft	691
Tree, Rem, 6 inch to 18 inch	Ea	1
Relocation and Site Cleanup	Dlr	15000

3 WORKING DAYS!
BEFORE YOU DIG
CALL MISS DIG
800-482-7171
(TOLL FREE)

LEGEND			
EXISTING FENCE	EXISTING TELEPHONE	EXISTING RD. & GRADE	PROPOSED CONTOUR
CONTOUR	GAS LINE	LEFT EDGE/RD. GRADE	847
STORM SEWER & CAT. BAS.	CABLE T.V.	RIGHT EDGE/RD. GRADE	
UTILITY POLE & GUY	ELECTRIC	CURB & GUTTER	
LIGHT POLE	SIGN	EDGE/GRAVEL	

SEE ABOVE	XXX					
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY
			ORIGINAL ISSUE	XXX	XXX	XXX

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR	
EAST MEDICAL CENTER DRIVE PATHWAY GENERAL NOTES	
SCALE NONE	0 INCH 1
DRAWING NO. C2	
SHEET NO. 2 OF 10	
APPROVED BY _____	



BENCHMARK #1: TOP OF SW TRAFFIC SIGNAL
POLE BASE BOLT AT THE SW CORNER OF
FULLER AVENUE AND MAIDEN LANE/E.
MEDICAL CENTER DRIVE. ELEV = 790.619'

3 WORKING DAYS!
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(TOLL FREE)

LEGEND

EXISTING FENCE	CONTOUR	STORM SEWER & CAT. BAS.	UTILITY POLE & GUY	LIGHT POLE
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EXISTING TELEPHONE	EXISTING RD. & GRADE	PROPOSED CONTOUR
--- ---	--- ---	--- ---
GAS LINE	LEFT EDGE/RD. GRADE	
CABLE T.V.	RIGHT EDGE/RD. GRADE	
ELECTRIC	CURB & GUTTER	
SIGN	EDGE/GRAVEL	

SEE ABOVE	XXX				
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

**EAST MEDICAL CENTER DRIVE
PATHWAY
EXISTING CONDITIONS**

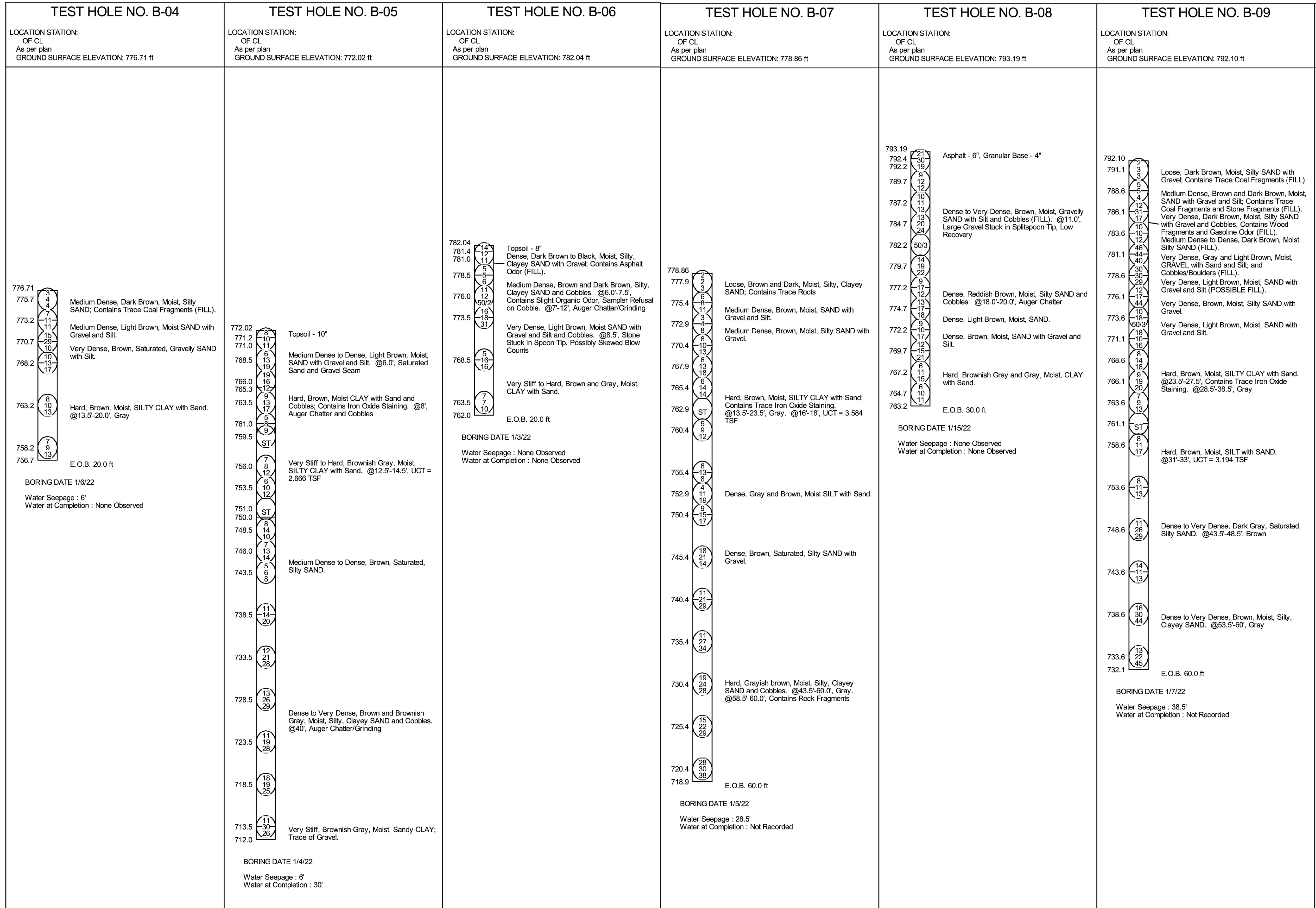
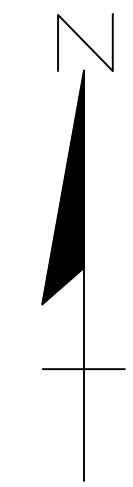
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DRAWING NO. TS1

SHEET NO. 3 OF 10

APPROVED BY: _____

LEIGH C. MERRILL P.E., MICHIGAN NO. 6201061025



3 WORKING DAYS
BEFORE YOU DIG
CALL MISS DIG
 800-482-7171
 (TOLL FREE)

LEGEND

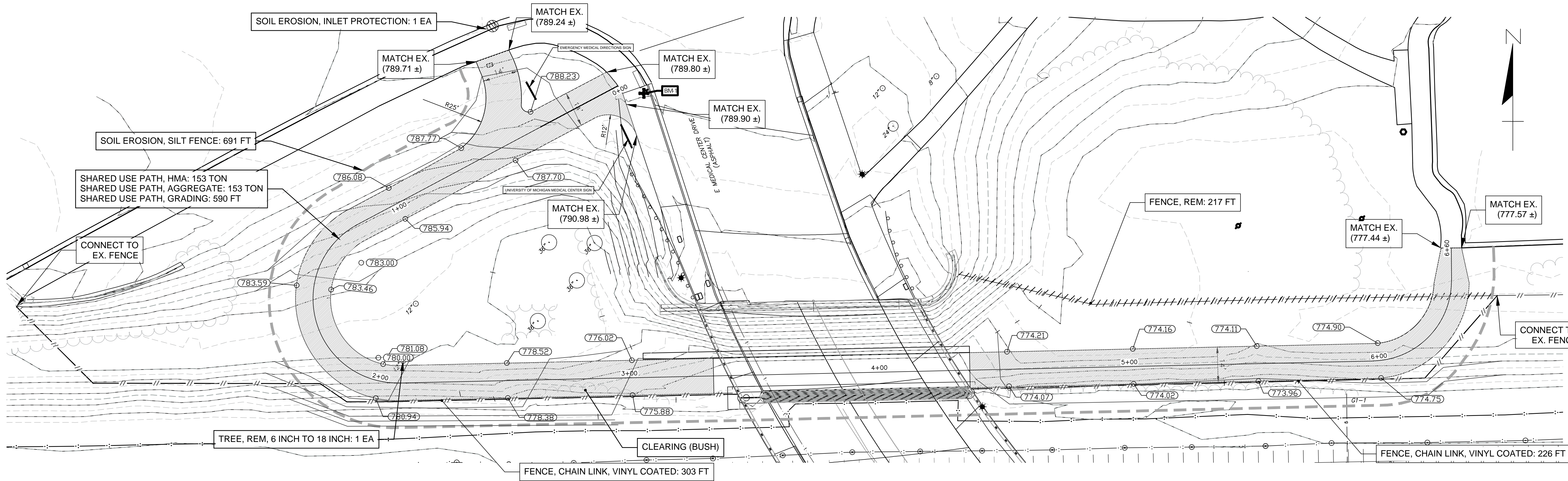
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CONTOUR	---847---	GAS LINE	---G---	LEFT EDGE/RD. GRADE	=====
STORM SEWER & CAT. BAS.	---r---	CABLE T.V.	---TV---	RIGHT EDGE/RD. GRADE	=====
UTILITY POLE & GUY	---o---	ELECTRIC	---e---	CURB & GUTTER	=====
LIGHT POLE	---* ---</td <td>SIGN</td> <td>---β---</td> <td>EDGE/GRAVEL</td> <td>=====</td>	SIGN	---β---	EDGE/GRAVEL	=====

SEE GENERAL PLAN OF SITE AND STRUCTURE SHEETS FOR SOIL BORING LOCATIONS.

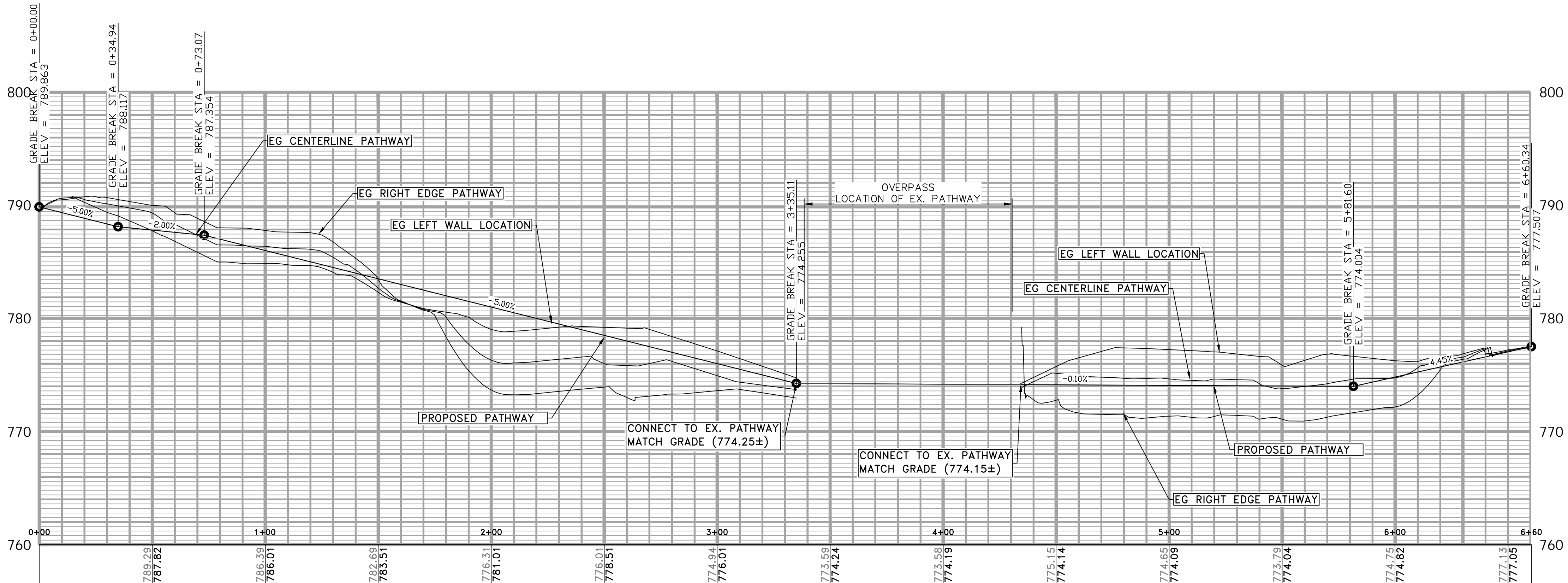
PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR	
EAST MEDICAL CENTER DRIVE PATHWAY SOIL BORINGS	
SCALE NONE	0 INCH 1
DRAWING NO. SB1	
SHEET NO. 4 OF 10	

SEE ABOVE	XXX	REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY
BENCH MARK	SURVEY BOOK		ORIGINAL ISSUE	XXX	XXX	XXX

APPROVED BY: _____
 LEIGH C. MERRILL P.E., MICHIGAN NO. 6201061025



ESTIMATED QUANTITIES		
DESCRIPTION	UNITS	QUANTITY
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Soil Erosion, Inlet Protection	Ea	1
Soil Erosion, Silt Fence	Ft	691
Tree, Rem, 6 inch to 18 inch	Ea	1



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LEGEND	
EXISTING FENCE	EXISTING TELEPHONE
CONTOUR	GAS LINE
STORM SEWER & CAT. BAS.	CABLE T.V.
UTILITY POLE & GUY	ELECTRIC
LIGHT POLE	SIGN
EXISTING RD. @ GRADE	PROPOSED CONTOUR
LEFT EDGE/RD. GRADE	SILT FENCE
RIGHT EDGE/RD. GRADE	INLET PROTECTION
CURB & GUTTER	REMOVAL
EDGE/GRAVEL	PROPOSED FENCE

SEE ABOVE	XXX	REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY
BENCH MARK	SURVEY BOOK		ORIGINAL ISSUE	XXX	XXX	XXX

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

**EAST MEDICAL CENTER DRIVE
 PATHWAY
 PROPOSED PLAN AND PROFILE**

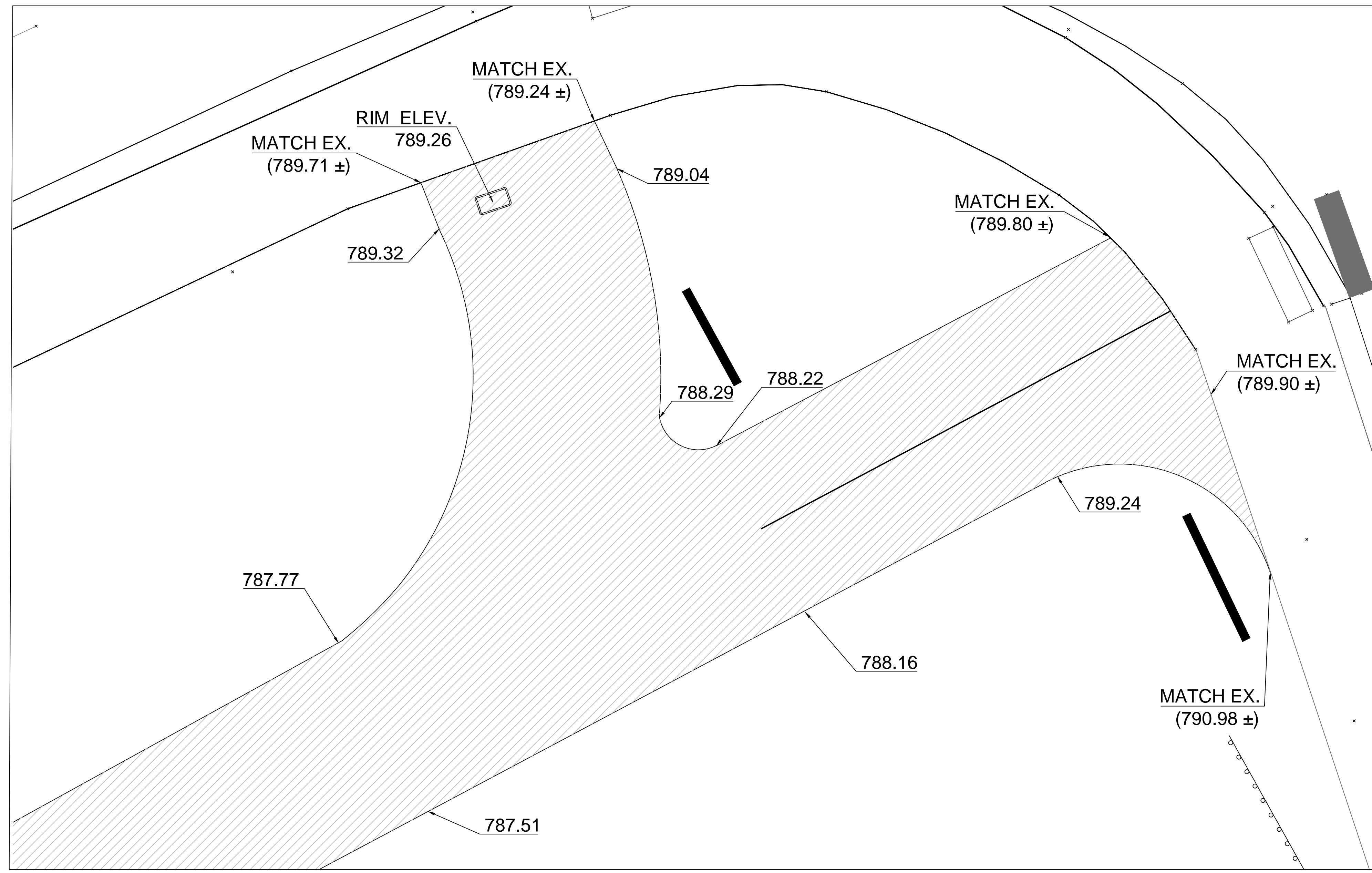
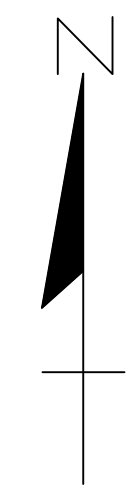
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DRAWING NO. R1

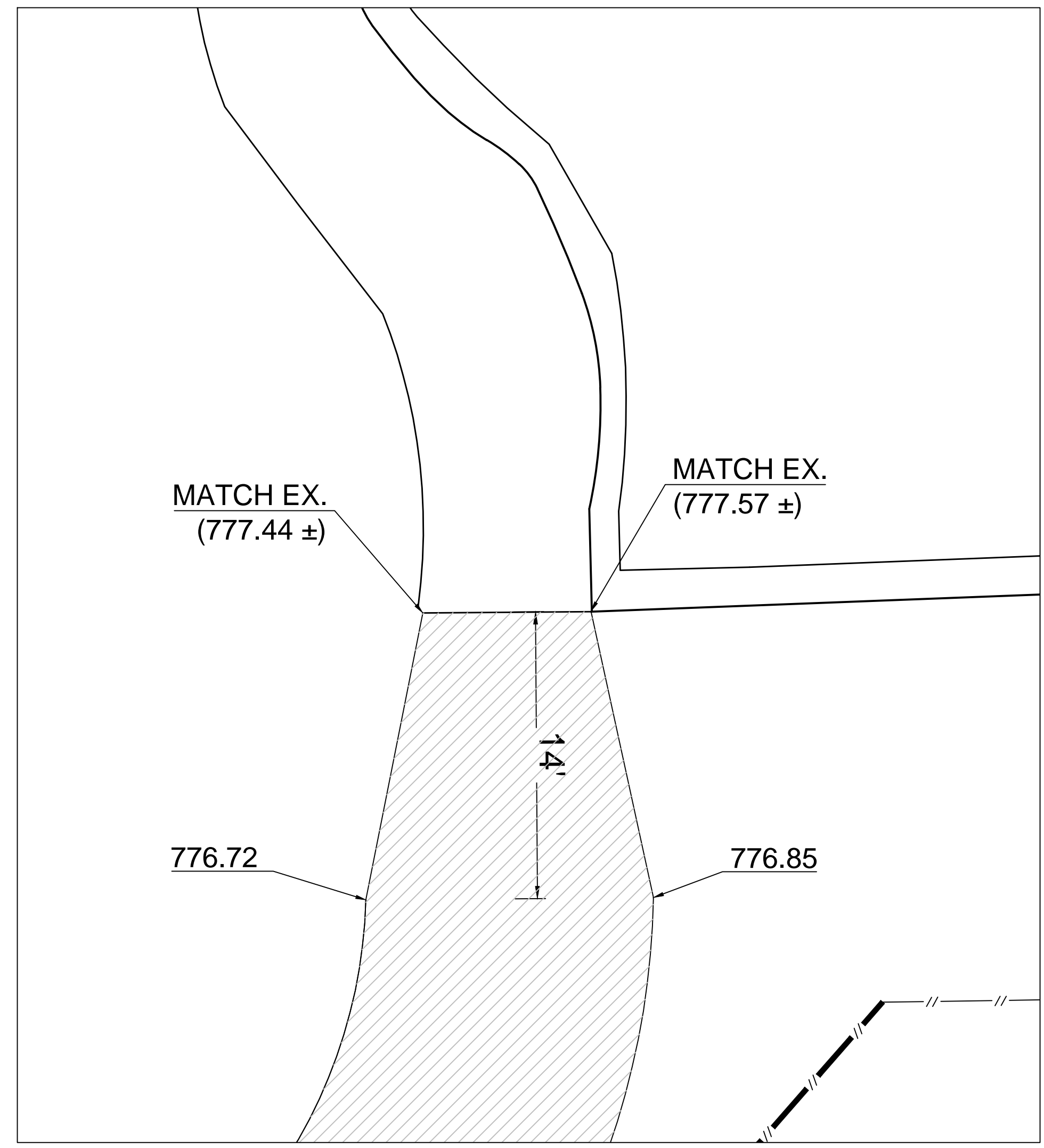
SHEET NO. 5 OF 10

APPROVED BY: _____

LEIGH C. MERRILL P.E., MICHIGAN NO. 6201061025



WEST END OF PATHWAY



EAST END OF PATHWAY

3 WORKING DAYS
BEFORE YOU DIG
CALL MISS DIG
800-482-7171
(TOLL FREE)

LEGEND

EXISTING FENCE	EXISTING TELEPHONE	EXISTING RD. @ GRADE	PROPOSED CONTOUR
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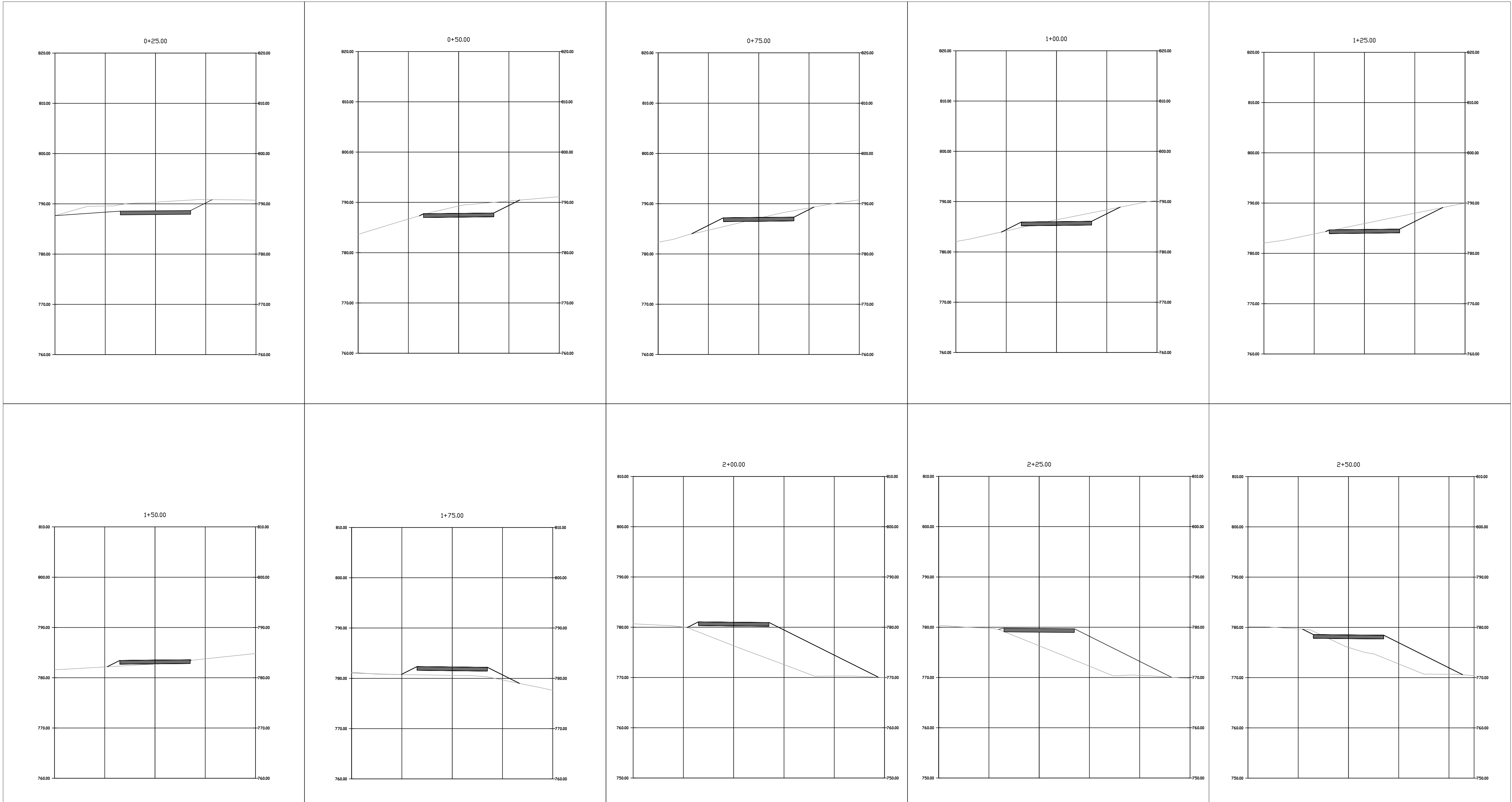
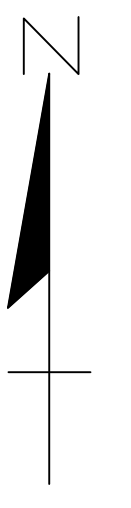
SEE ABOVE	XXX				
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

EAST MEDICAL CENTER DRIVE
PATHWAY
DETAIL GRADES

SCALE 1"=5'	INCH 0 1
DRAWING NO. GD1	
SHEET NO. 6 OF 10	

APPROVED BY: _____
LEIGH C. MERRILL P.E., MICHIGAN NO. 6201061025



3 WORKING DAYS
BEFORE YOU DIG
CALL MISS DIG
800-482-7171
(TOLL FREE)

LEGEND

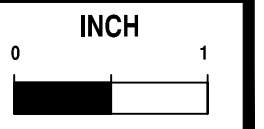
- | | | | | | | | |
|---------------------------|-----------------|--------------------|----------|------------------------|-------|------------------|-----------|
| EXISTING FENCE | --- --- --- --- | EXISTING TELEPHONE | ---t--- | EXISTING RD. @ GRADE | ===== | PROPOSED CONTOUR | ---847--- |
| - CONTOUR | ---847--- | - GAS LINE | ---g--- | - LEFT EDGE/RD. GRADE | ===== | | |
| - STORM SEWER & CAT. BAS. | ---r--- | - CABLE T.V. | ---tv--- | - RIGHT EDGE/RD. GRADE | ===== | | |
| - UTILITY POLE & GUY | ---o/h--- | - ELECTRIC | ---e--- | - CURB & GUTTER | ===== | | |
| - LIGHT POLE | ---* | - SIGN | ---b--- | - EDGE/GRAVEL | ===== | | |

SEE ABOVE	XXX					
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY	CH. BY
			ORIGINAL ISSUE	XXX	XXX	XXX

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

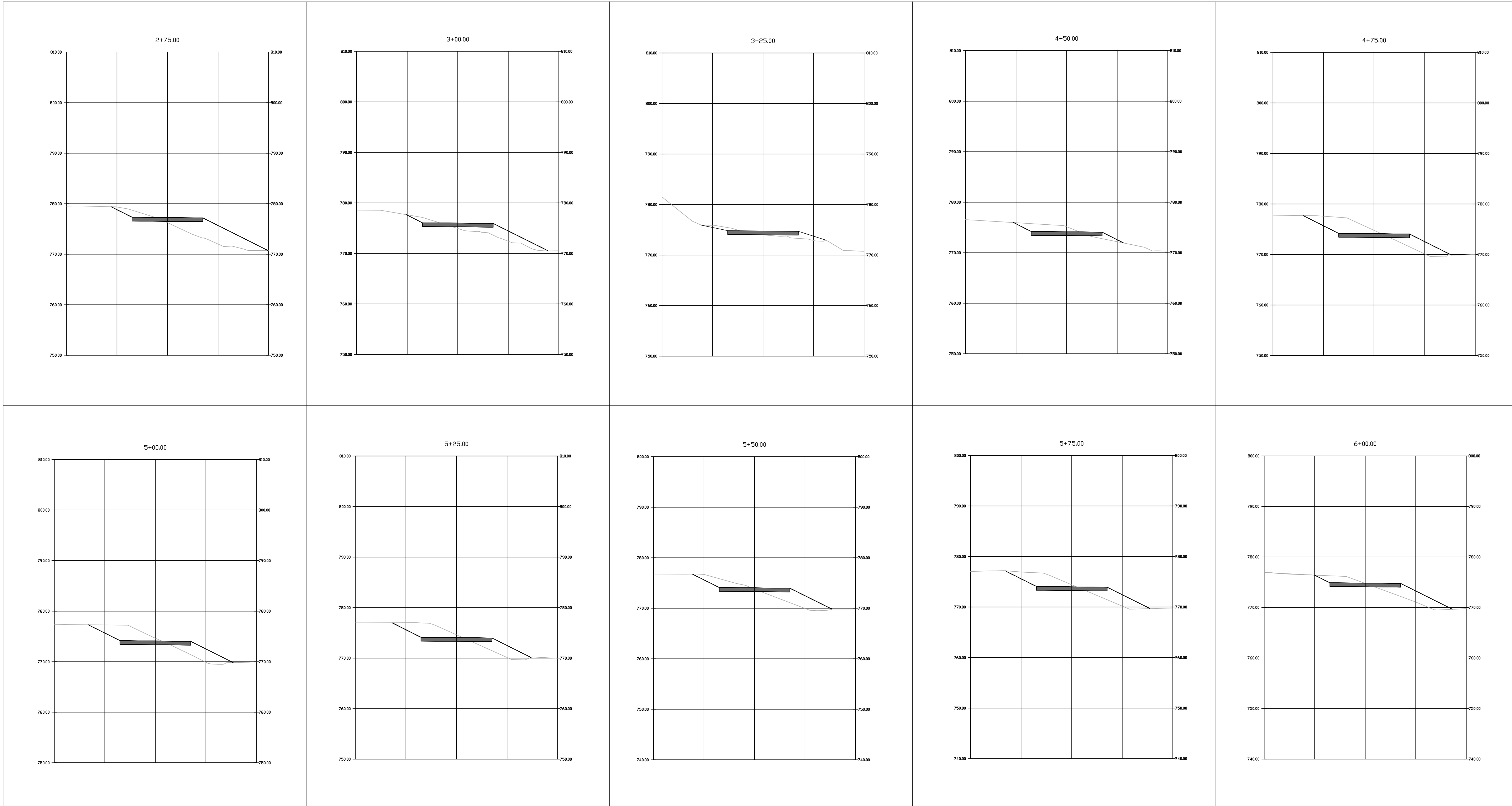
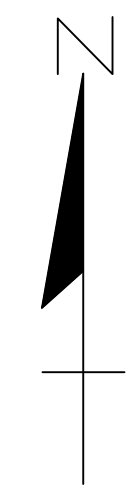
**EAST MEDICAL CENTER DRIVE
PATHWAY CROSS SECTIONS**

SCALE
1"=10'



DRAWING NO.
GD2

APPROVED BY _____
SHEET NO. 7 OF 10



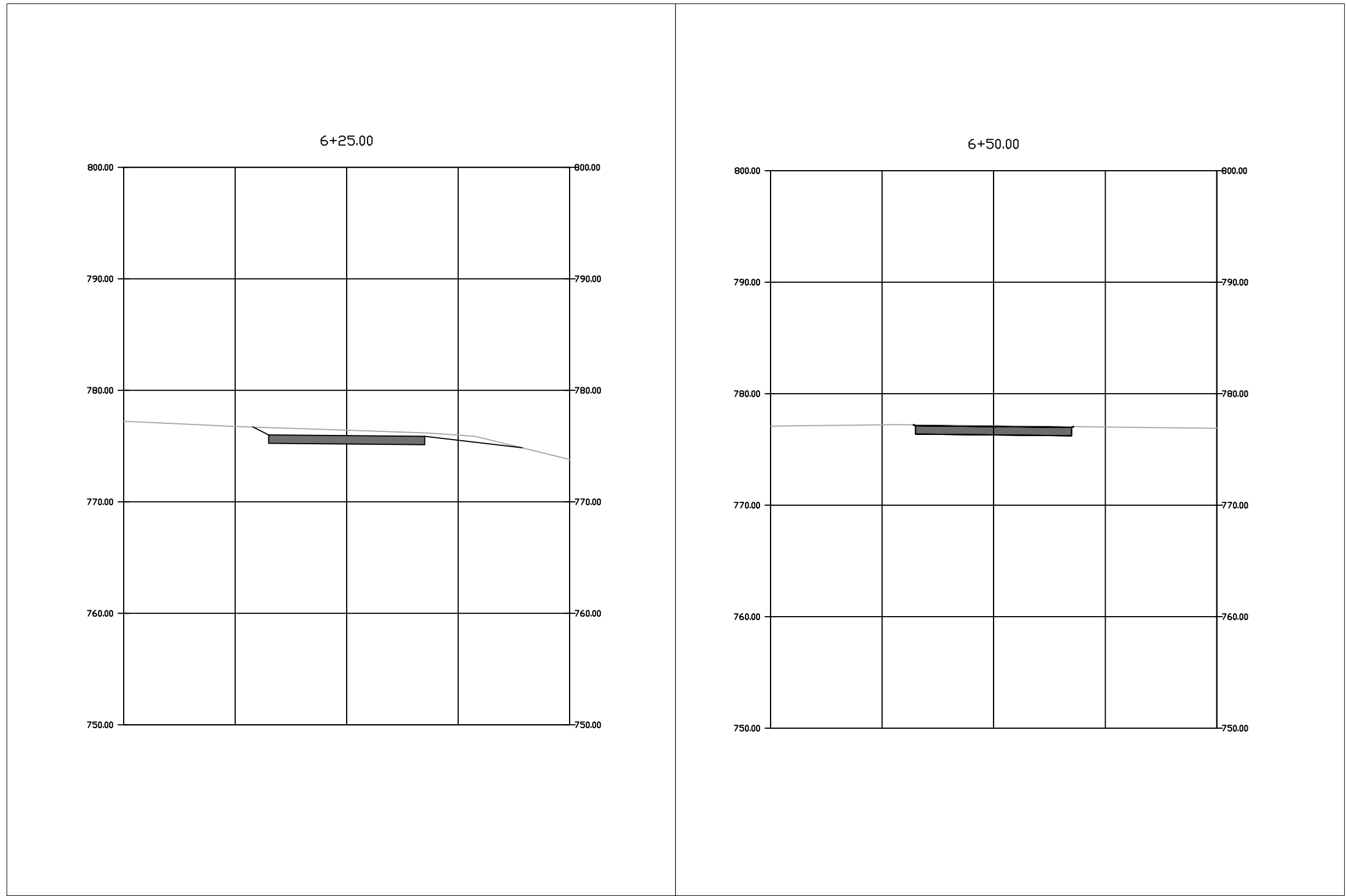
3 WORKING DAYS!
BEFORE YOU DIG
CALL MISS DIG
800-482-7171
(TOLL FREE)

LEGEND

- | | | | | | | | |
|---------------------------|-------------|--------------------|-------------|------------------------|-------|------------------|-------------|
| EXISTING FENCE | --- --- --- | EXISTING TELEPHONE | --- --- --- | EXISTING RD. @ GRADE | ===== | PROPOSED CONTOUR | --- --- --- |
| - CONTOUR | --- --- --- | - GAS LINE | --- --- --- | - LEFT EDGE/RD. GRADE | ===== | | |
| - STORM SEWER & CAT. BAS. | --- --- --- | - CABLE T.V. | --- --- --- | - RIGHT EDGE/RD. GRADE | ===== | | |
| - UTILITY POLE & GUY | --- --- --- | - ELECTRIC | --- --- --- | - CURB & GUTTER | ===== | | |
| - LIGHT POLE | --- --- --- | - SIGN | --- --- --- | - EDGE/GRAVEL | ===== | | |

SEE ABOVE	XXX				
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY
			ORIGINAL ISSUE	XXX	XXX
				XXX	XXX

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR	
EAST MEDICAL CENTER DRIVE PATHWAY CROSS SECTIONS	
SCALE 1"=10'	INCH 0 1
DRAWING NO. GD3	
SHEET NO. 8 OF 10	
APPROVED BY _____	



3 WORKING DAYS
BEFORE YOU DIG
 CALL MISS DIG
 800-482-7171
 (TOLL FREE)

LEGEND

EXISTING FENCE	--- --- ---	EXISTING TELEPHONE	--- ---	EXISTING RD. @ GRADE	=====	PROPOSED CONTOUR	— 847 —
- CONTOUR	--- 847 ---	- GAS LINE	--- g ---	- LEFT EDGE/RD. GRADE	=====		
- STORM SEWER & CAT. BAS.	--- r ---	- CABLE T.V.	--- tv ---	- RIGHT EDGE/RD. GRADE	=====		
- UTILITY POLE & GUY	o/h ---	- ELECTRIC	--- e ---	- CURB & GUTTER	=====		
- LIGHT POLE	* ---	- SIGN	--- b ---	- EDGE/GRAVEL	=====		

SEE ABOVE	XXX				
BENCH MARK	SURVEY BOOK	REV. NO.	DESCRIPTION	DATE	DR. BY
			ORIGINAL ISSUE	XXX	XXX
				XXX	XXX

PROJECT MANAGEMENT - PUBLIC SERVICES - CITY OF ANN ARBOR

**EAST MEDICAL CENTER DRIVE
 PATHWAY CROSS SECTIONS**

SCALE 1"=10'	INCH 0 1
DRAWING NO. GD4	
SHEET NO. <u>9</u> OF <u>10</u>	

APPROVED BY _____
 LEIGH C. MERRIL P.E., MICHIGAN NO. 6201061025

Engineer's Opinion of Costs

Project Number: 2141736302
Estimate Number: 1: East Medical Center Drive Pathway
Project Type: Miscellaneous
Location: Pathway
 Ann Arbor
Description: Pathway

Project Engineer: DLZ
Date Created: 5/17/2022
Date Edited: 11/23/2022
Fed/State #:
Fed Item:
Control Section:

Line	Pay Item	Description	Quantity	Units	Unit Price	Total
Category: 0003 100% City of Ann Arbor						
0001	1027051	_ 20% Contingency	1.000	LSUM	\$40,000.00	\$40,000.00
0002	1047051	_ Certified Payroll Compliance and Reporting	1.000	LSUM	\$5,000.00	\$5,000.00
0003	1047051	_ Project Supervision	1.000	LSUM	\$6,000.00	\$6,000.00
0004	1077060	_ Relocation and Site Cleanup	15,000.000	Dlr	\$1.00	\$15,000.00
0005	2020004	Tree, Rem, 6 inch to 18 inch	1.000	Ea	\$2,000.00	\$2,000.00
0006	2040025	Fence, Rem	217.000	Ft	\$5.00	\$1,085.00
0007	2050010	Embankment, CIP	535.000	Cyd	\$30.00	\$16,050.00
0008	2050016	Excavation, Earth	372.000	Cyd	\$30.00	\$11,160.00
0009	2050031	Non Haz Contaminated Material Handling and Disposal, LM	50.000	Cyd	\$300.00	\$15,000.00
0010	2080036	Erosion Control, Silt Fence	691.000	Ft	\$5.00	\$3,455.00
0011	2087050	_ Erosion Control, Inlet Protection	1.000	Ea	\$150.00	\$150.00
0012	8060010	Shared use Path, Aggregate	153.000	Ton	\$50.00	\$7,650.00
0013	8060030	Shared use Path, Grading	590.000	Ft	\$25.00	\$14,750.00
0014	8060040	Shared use Path, HMA	153.000	Ton	\$120.00	\$18,360.00
0015	8082007	Fence, Chain Link, 120 inch, Vinyl Coated	529.000	Ft	\$120.00	\$63,480.00
0016	8120170	Minor Traf Devices	1.000	LSUM	\$5,000.00	\$5,000.00
0017	8507051	_ General Conditions, Max \$16,000	1.000	LSUM	\$18,000.00	\$18,000.00

Category 0003 Total: \$242,140.00

Estimate Total: \$242,140.00